

VICINITY MAP
N.T.S.

SPECIFICATIONS LIST

- SM&E JANUARY 23, 2019 REPORT OF PAVEMENT EXPLORATION PROJECT NUMBER #1463-18-054.
- ADDITIONAL SPECIFICATIONS SHALL BE AS NOTED ON THESE PLANS.

CastlesEngineering

SITE CONSTRUCTION PLANS FOR: CCU PARKING LOT EE RESURFACING TOWNSHIP CIRCLE RECLAMATION

STATE PROJECT #H17-N120-MJ

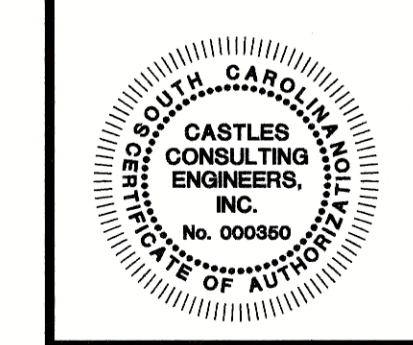
PROJECT SUMMARY:	
MILL & OVERLAY OF EXISTING ROADWAYS COASTAL CAROLINA UNIVERSITY CONWAY, SOUTH CAROLINA	
TAX PARCEL:	TMS #151-00-01-099 PIN #383-00-000-375
TOTAL PROJECT AREA - 80.3 ACRES	
CL LENGTH OF ROAD - 2,480 +/- LF	

ENGINEERING / PLANNING
LAND SURVEYING
2024 Corporate Centre Drive
Suite 102
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COVER SHEET
CCU PARKING LOT EE RESURFACING
STATE PROJECT #H17-N120-MJ
COASTAL CAROLINA UNIVERSITY
CITY OF CONWAY, HORRY COUNTY, SOUTH CAROLINA

SCALE:	N.T.S
DESIGNED BY:	JRP
DRAWN BY:	TCM
CHECKED BY:	RLC
DATE ISSUED:	07/15/19

Number	Date	Revisions
1		
2		
3		
4		
5		



SHEET NUMBER:
C1.0
STATE PN#: H17-N120-MJ
PROJECT#: 18.096

FLOOD ZONE INFORMATION

THIS PROPERTY IS LOCATED IN FLOOD ZONE X ACCORDING TO FEMA MAP NO. 45051C0517 H, DATED 09-23-89 AND IS SUBJECT TO VERIFICATION BY THE COUNTY FEMA OFFICER.

GEOTECHNICAL CONSULTANT

S&ME
1330 HIGHWAY 501 BUSINESS
CONWAY, SC 29526
PHONE: (843) 347-7800

OWNER / DEVELOPER

COASTAL CAROLINA UNIVERSITY
P.O. BOX 261954
CONWAY, SC 29528

LAND SURVEYOR

CASTLES ENGINEERING
2024 CORPORATE CENTRE DRIVE, SUITE 102
MYRTLE BEACH, SC 29577
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SHEET INDEX

DESCRIPTION	SHEET
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SITE PLAN 1 AND SITE PLAN 2	C4.0 - C5.0
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HORIZONTAL DATUM: (NAD) 2011
VERTICAL DATUM: NAVD 88
BENCHMARKS: CCU CAMPUS MONUMENT SYSTEM SEE SITE 1 PLAN C4.0

3 DAYS BEFORE DIGGING IN
SOUTH CAROLINA

CALL 1-800-922-0983
PALMETTO UTILITY PROTECTION SERVICE

CONTRACTOR'S RESPONSIBILITIES:

CONTRACTOR SHALL BE KNOWLEDGEABLE WITH LOCAL, CITY, COUNTY, STATE, AND FEDERAL REGULATIONS AS THEY MAY PERTAIN TO THIS DEVELOPMENT AND SHALL ADHERE TO THESE REGULATIONS.

UNDERGROUND UTILITIES:

INFORMATION REGARDING THE PRESENCE, SIZE, CHARACTER, AND LOCATION OF ANY UNDERGROUND UTILITY AND/OR STRUCTURE SHOWN ON THIS PLAN IS APPROXIMATE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO HAVE ALL UNDERGROUND UTILITIES LOCATED.

NO.	DATE	DESCRIPTION	SHEETS AFFECTED	BY

SCOPE OF ASPHALT RECLAMATION/REPLACEMENT (11,035 Sq. Yds.)

253 TOTAL PARKS (243 STANDARD STALLS) (10 HANDICAP STALLS)

NOTE:
1. FOR FULL DEPTH REPLACEMENT, HEAVY-DUTY SECTION WILL BE USED IN ALL DRIVEWAYS AND DRIVE-AISLES, LIGHT-DUTY SECTION WILL BE USED IN ALL PARKING STALLS.

**PURPOSE OF ISSUE:
CONSTRUCTION SET**

DWG NAME: P118096 CCU PARKING LOT EE RESURFACING DESIGN CAD DRAWINGS CONSTRUCTION SHEETS 18096 COVER NOTES AND DETAIL DWG

GRADING & STORM DRAINAGE

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THESE PLANS, CITY OF CONWAY STANDARD SPECIFICATIONS AND SCDHEC-OCRM STANDARDS.
- THE CONTRACTOR SHALL FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING UTILITIES PRIOR TO START OF CONSTRUCTION AND SHALL NOTIFY THE CONSTRUCTION MANAGER AND ENGINEER OF ANY CONFLICTS DISCOVERED. SHOULD THE CONTRACTOR PROCEED WITH CONSTRUCTION PRIOR TO DOING THIS AND ANY CONFLICTS OCCUR THEN THE CONTRACTOR WILL BE RESPONSIBLE FOR THE TOTAL COST TO REMEDY THE SITUATION INCLUDING ENGINEERING FEES. CONTRACTOR IS RESPONSIBLE FOR PROTECTING EXISTING UTILITIES (SHOWN OR NOT SHOWN) WITHIN SCOPE OF CONSTRUCTION. IF ANY EXISTING UTILITIES ARE DAMAGED, THE CONTRACTOR SHALL REPAIR THEM AT HIS OWN EXPENSE.
- THE CONTRACTOR SHALL MAINTAIN ADEQUATE SITE DRAINAGE DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL USE SILT FENCES (OR OTHER METHODS APPROVED BY THE ENGINEER, CITY OF CONWAY AND OCRM) AS REQUIRED TO PREVENT SILT AND CONSTRUCTION DEBRIS FROM MIGRATING ONTO ADJACENT PROPERTIES. CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL EROSION, CONSERVATION, AND SILTATION ORDINANCES. CONTRACTOR SHALL REMOVE ALL TEMPORARY EROSION CONTROL DEVICES UPON COMPLETION OF PERMANENT DRAINAGE FACILITIES AND THE ESTABLISHMENT OF A STAND OF GRASS OR OTHER GROWTH TO PREVENT EROSION.
- BEFORE ANY EARTHWORK IS DONE, THE CONTRACTOR SHALL STAKE OUT AND FLAG THE CLEARING LIMITS AND OTHER ITEMS ESTABLISHED BY THE PLANS. THE CONTRACTOR SHALL PROVIDE ALL NECESSARY ENGINEERING AND SURVEYING FOR LINE AND GRADE CONTROL POINTS RELATED TO EARTHWORK.
- CLEARING LIMITS SHALL BE, AT A MINIMUM, THE CLEARING REQUIRED IN ORDER TO FACILITATE THE WORK, OR TO PROVIDE FOR ADDITIONAL UTILITIES OR EASEMENTS AND SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- EXISTING CONTOURS AS SHOWN ON THIS PLAN WERE TAKEN FROM A FIELD TOPOGRAPHIC SURVEY PREPARED BY ASSOCIATED LAND SURVEYORS. CONTRACTOR SHALL CONTACT AND REFERENCE SAME BENCHMARK AS USED BY SURVEYOR.
- THE CONTRACTOR SHALL CLEAR AND GRUB THE SITE AND PLACE AND COMPACT ALL FILL PER THE PROJECT GEOTECHNICAL ENGINEER'S SPECIFICATIONS. WHEN PROVIDED, THE FILL MATERIAL TO BE USED SHALL BE APPROVED BY THE GEOTECHNICAL ENGINEER PRIOR TO PLACEMENT.
- ALL BACKFILL AND FILL SOILS SHOULD BE NON-PLASTIC AND GRANULAR IN NATURE. IF NO GEOTECHNICAL SPECIFICATIONS ARE AVAILABLE, SOILS SHOULD BE PLACED IN MAXIMUM EIGHT (8.0) INCH COMPACTED LIFTS. EACH LIFT SHOULD BE COMPACTED TO AT LEAST NINETY-FIVE PERCENT (95%) OF THE SOIL'S MODIFIED PROCTOR MAXIMUM DRY DENSITY (ASTM D1557).
- GRADING CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANIES FOR ANY REQUIRED UTILITY ADJUSTMENTS AND/OR RELOCATIONS.
- CONTRACTOR IS RESPONSIBLE FOR HAVING IN HIS POSSESSION ALL REQUIRED PERMITS AND APPROVALS PRIOR TO START OF CONSTRUCTION.
- REFER TO PLAN AND DETAIL SHEETS FOR EROSION CONTROL DEVICES TO BE INSTALLED PRIOR TO COMMENCING CONSTRUCTION.
- NO TREE SHALL BE REMOVED OR DAMAGED WITHOUT PRIOR AUTHORIZATION OF THE OWNER OR OWNER'S REPRESENTATIVE. EXISTING TREES TO BE SAVED AS SHOWN ON THE DRAWINGS SHALL BE PRESERVED.
- CONTRACTOR IS RESPONSIBLE FOR PROPERLY DISPOSING OF UNSUITABLE MATERIAL. ALL SUITABLE MATERIAL SHALL BE STOCKPILED AT OWNER'S DIRECTION.
- ALL EXCAVATION IS UNCLASSIFIED AND SHALL INCLUDE ALL MATERIALS ENCOUNTERED. UNSAVABLE EXCAVATED MATERIAL AND ALL WASTE RESULTING FROM SITE CLEARING AND GRUBBING SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR AT HIS EXPENSE.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITE SAFETY DURING ALL PHASES OF CONSTRUCTION.
- ALL DRAINAGE PIPE SHALL BE CLASS III RCP PER SECTION 714 OF DOT STANDARD SPECIFICATIONS, UNLESS OTHERWISE NOTED.
- RCP DENOTES REINFORCED CONCRETE PIPE, CLASS III UNLESS OTHERWISE NOTED
- FOR PIPE JOINTS, CATCH BASINS AND ALL OTHER DRAINAGE STRUCTURES CONTRACTOR TO USE TYPE M OR S MORTAR
- JUTE MATTING MUST BE INSTALLED ON ANY SLOPE GREATER THAN 3 (HORIZONTAL) TO 1 (VERTICAL) TO ENSURE SLOPE STABILITY
- DRAINAGE ARROWS ON PLAN ARE SHOWN AS REFERENCE TO ASSIST THE CONTRACTOR IN MAINTAINING POSITIVE DRAINAGE.

CONSTRUCTION SEQUENCE

- OBTAIN REQUIRED CITY LICENSES
- MOBILIZATION ON-SITE
- LOCATE ALL UTILITIES
- CONTRACTOR TO SUBMIT A TRAFFIC CONTROL PLAN TO OWNER AND ENGINEER
- CALL FOR ON-SITE INSPECTION BY ENGINEERING INSPECTOR. AFTER APPROVAL,
- DEMOLITION OF EXISTING FACILITIES IF ANY.
- PERFORM RECLAMATION OF EXISTING PAVEMENT AREAS IN ACCORDANCE WITH THE PHASING PLAN
- INSTALL PAVEMENT MARKINGS PER PLAN
- SITE CLEANUP / DEMOBILIZATION.

EROSION CONTROL NOTES

- IF NECESSARY, SLOPES, WHICH EXCEED EIGHT (8) VERTICAL FEET SHOULD BE STABILIZED WITH SYNTHETIC OR VEGETATIVE MATS. IN ADDITION TO HYDROSEEDING, IT MAY BE NECESSARY TO INSTALL TEMPORARY SLOPE DRAINS DURING CONSTRUCTION. TEMPORARY BERMS MAY BE NEEDED UNTIL THE SLOPE IS BROUGHT TO GRADE.
- STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN FOURTEEN (14) DAYS AFTER WORK HAS CEASED, EXCEPT AS STATED BELOW.
 - WHERE STABILIZATION BY THE 14TH DAY IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS STABILIZATION MEASURES MUST BE INITIATED AS SOON AS PRACTICABLE.
 - WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-DISTURBING ACTIVITIES WILL BE RESUMED WITHIN 14 DAYS, TEMPORARY STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE.
- ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSPECTED ONCE CALENDAR EVERY WEEK. IF PERIODIC INSPECTION OR OTHER INFORMATION INDICATES THAT A BMP HAS BEEN INAPPROPRIATELY, OR INCORRECTLY, THE PERMITTEE MUST ADDRESS THE NECESSARY REPLACEMENT OR MODIFICATION REQUIRED TO CORRECT THE BMP WITHIN 48 HOURS OF IDENTIFICATION.
- PROVIDE SILT FENCE AND/OR OTHER CONTROL DEVICES, AS MAY BE REQUIRED, TO CONTROL SOIL EROSION DURING UTILITY CONSTRUCTION. ALL DISTURBED AREAS SHALL BE CLEANED, GRADED, AND STABILIZED WITH GRASSING IMMEDIATELY AFTER THE UTILITY INSTALLATION. FILL, COVER, AND TEMPORARY SEEDING AT THE END OF EACH DAY ARE REMOVED. IF WATER IS ENCOUNTERED WHILE TRENCHING, THE WATER SHOULD BE FILTERED TO REMOVE SEDIMENT BEFORE BEING PUMPED BACK INTO ANY WATERS OF THE STATE.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION UNTIL THE COMPLETION OF ALL CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.
- THE CONTRACTOR MUST TAKE NECESSARY ACTION TO MINIMIZE THE TRACKING OF MUD ONTO PAVED ROADWAY(S) FROM CONSTRUCTION AREAS AND THE GENERATION OF DUST. THE CONTRACTOR SHALL DAILY REMOVE MUD/SOIL FROM PAVEMENT, AS MAY BE REQUIRED.
- RESIDENTIAL SUBDIVISIONS REQUIRE EROSION CONTROL FEATURES FOR INFRASTRUCTURE AS WELL AS FOR INDIVIDUAL LOT CONSTRUCTION. INDIVIDUAL PROPERTY OWNERS SHALL FOLLOW THESE PLANS DURING CONSTRUCTION OR OBTAIN APPROVAL OF AN INDIVIDUAL PLAN IN ACCORDANCE WITH S.C. REG. 72-300 ET SEQ. AND SCR10000.
- TEMPORARY DIVERSION BERMS AND/OR DITCHES WILL BE PROVIDED AS NEEDED DURING CONSTRUCTION TO PROTECT WORK AREAS FROM UPSLOPE RUNOFF AND/OR TO DIVERT SEDIMENT-LADEN WATER TO APPROPRIATE TRAPS OR STABLE OUTLETS.
- ALL WATERS OF THE STATE (WOS), INCLUDING WETLANDS, ARE TO BE FLAGGED OR OTHERWISE CLEARLY MARKED IN THE FIELD. A DOUBLE ROW OF SILT FENCE IS TO BE INSTALLED IN ALL AREAS WHERE A 50-FOOT BUFFER CANT BE MAINTAINED BETWEEN THE DISTURBED AREA AND ALL WOS. A 10-FOOT BUFFER SHOULD BE MAINTAINED BETWEEN THE LAST ROW OF SILT FENCE AND ALL WOS.
- LITTER, CONSTRUCTION DEBRIS, OILS, FUELS, AND BUILDING PRODUCTS WITH SIGNIFICANT POTENTIAL FOR IMPACT (SUCH AS STOCKPILES OF FRESHLY TREATED LUMBER) AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORM WATER MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE IN STORM WATER DISCHARGES.
- A COPY OF THE SWPPP, INSPECTIONS RECORDS, AND RAINFALL DATA MUST BE RETAINED AT THE CONSTRUCTION SITE OR A NEARBY LOCATION EASILY ACCESSIBLE DURING NORMAL BUSINESS HOURS, FROM THE DATE OF COMMENCEMENT OF CONSTRUCTION ACTIVITIES TO THE DATE THAT FINAL STABILIZATION IS REACHED. 12. INITIATE STABILIZATION MEASURES ON ANY EXPOSED STEEP SLOPE (3:1 V OR GREATER) WHERE LAND-DISTURBING ACTIVITIES HAVE PERMANENTLY OR TEMPORARILY CEASED, AND WILL NOT RESUME FOR A PERIOD OF 7 CALENDAR DAYS.
- MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL.
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM EQUIPMENT AND VEHICLE WASHING, WHEEL WASH WATER, AND OTHER WASH WATERS. WASH WATERS MUST BE TREATED IN A SEDIMENT BASIN OR ALTERNATIVE CONTROL THAT PROVIDES EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE;
- MINIMIZE THE DISCHARGE OF POLLUTANTS FROM DEWATERING OF TRENCHES AND EXCAVATED AREAS. THESE DISCHARGES ARE TO BE ROUTED THROUGH APPROPRIATE BMPS (SEDIMENT BASIN, FILTER BAG, ETC.).
- THE FOLLOWING DISCHARGES FROM SITES ARE PROHIBITED:
 - WASTEWATER FROM WASHOUT OF CONCRETE, UNLESS MANAGED BY AN APPROPRIATE CONTROL;
 - WASTEWATER FROM WASHOUT AND CLEANOUT OF STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS AND OTHER CONSTRUCTION MATERIALS;
 - FUELS, OILS, OR OTHER POLLUTANTS USED IN VEHICLE AND EQUIPMENT OPERATION AND MAINTENANCE; AND
 - SOAPS OR SOLVENTS USED IN VEHICLE AND EQUIPMENT WASHING.
- AFTER CONSTRUCTION ACTIVITIES BEGIN, INSPECTIONS MUST BE CONDUCTED AT A MINIMUM OF AT LEAST ONCE EVERY CALENDAR WEEK AND MUST BE CONDUCTED UNTIL FINAL STABILIZATION IS REACHED ON ALL AREAS OF THE CONSTRUCTION SITE.
- IF EXISTING BMPS NEED TO BE MODIFIED OR IF ADDITIONAL BMPS ARE NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THIS PERMIT AND/OR SC'S WATER QUALITY STANDARDS, IMPLEMENTATION MUST BE COMPLETED BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE. IF IMPLEMENTATION BEFORE THE NEXT STORM EVENT IS IMPRACTICABLE, THE SITUATION MUST BE DOCUMENTED IN THE SWPPP AND ALTERNATIVE BMPS MUST BE IMPLEMENTED AS SOON AS REASONABLY POSSIBLE.
- A PRE-CONSTRUCTION CONFERENCE MUST BE HELD FOR EACH CONSTRUCTION SITE WITH AN APPROVED ON-SITE SWPPP PRIOR TO THE IMPLEMENTATION OF CONSTRUCTION ACTIVITIES. FOR NON-LINEAR PROJECTS THAT DISTURB 10 ACRES OR MORE THIS CONFERENCE MUST BE HELD ON-SITE UNLESS THE DEPARTMENT HAS APPROVED OTHERWISE.

PAVING

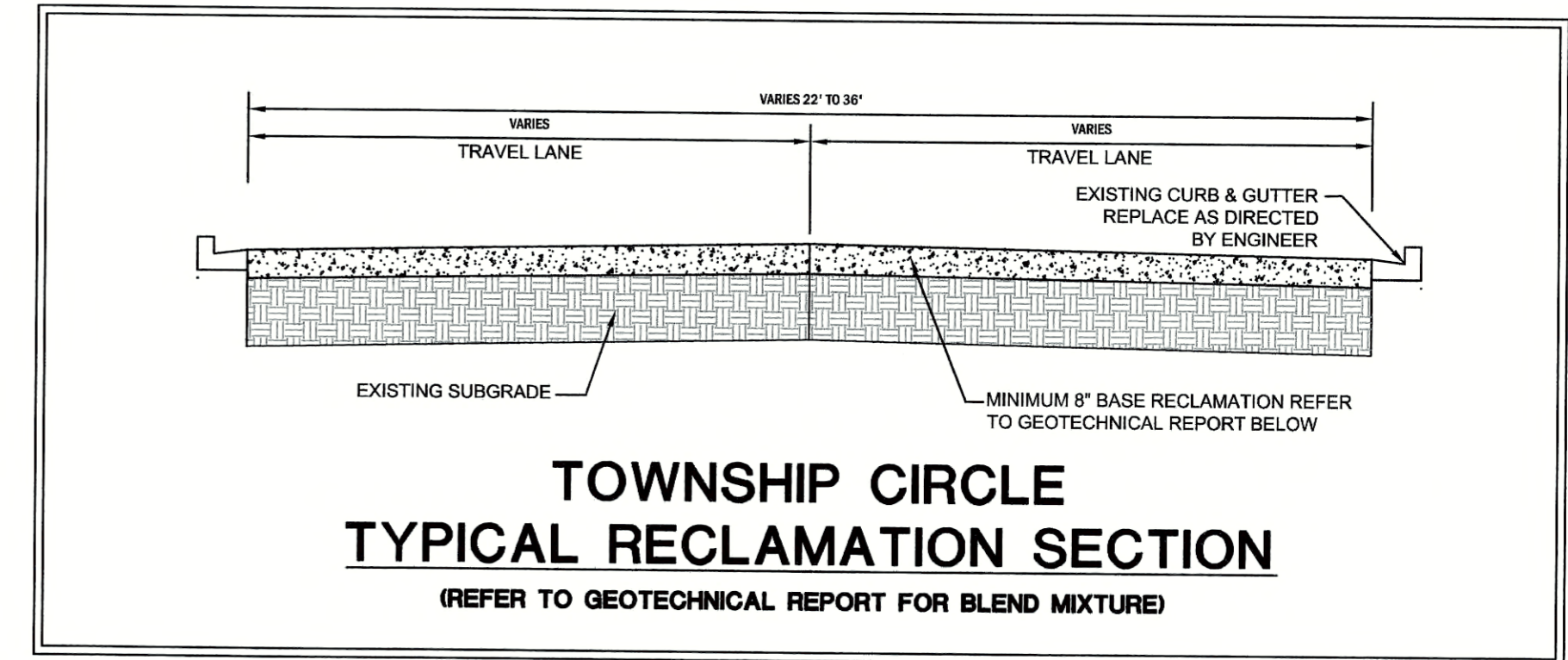
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THESE PLANS SCDOT STANDARD SPECIFICATIONS, LATEST EDITION, CITY OF CONWAY SPECIFICATIONS, AND GEOTECHNICAL REPORT (WHEN PROVIDED).
- UPON COMPLETION OF PAVING, CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING PAVEMENT CORE DATA AS REQUESTED BY CITY OF CONWAY OR THE ENGINEER.
- PRIME COAT AND TACK COAT APPLICATION TO BE IN ACCORDANCE WITH SCDOT STANDARD SPECIFICATIONS, IF REQUIRED BY GEOTECHNICAL ENGINEER.
- THE EXISTING OR MILLED PAVEMENT SURFACES SHOULD BE THOROUGHLY SWEEPED AND SCRAPPED CLEAN OF LOOSE ASPHALT, DELETERIOUS MATERIALS AND MAINTAINED UNTIL NEW ASPHALT MIXTURE IS PLACED.
- MAX LIFT IN ANY PAVEMENT AREA SHALL BE 3".
- ASPHALT SECTION PROVIDED AS A MINIMUM, VARIOUS LOCATIONS WILL REQUIRE ASPHALT WEDGE.

PAVEMENT MARKING

- ALL SIGNS, PAVEMENT MARKINGS, AND OTHER TRAFFIC CONTROL DEVICES ON PUBLIC STREETS SHALL CONFORM TO THE SCDOT STANDARDS FOR ROADWAY CONSTRUCTION AND THE SCDOT SUPPLEMENT FOR HIGHWAY CONSTRUCTION.
- UNLESS OTHERWISE DIRECTED, PAVEMENT MARKINGS SHALL BE PAINT OR AS DIRECTED BY THE OWNER. PAINT IS PERMITTED FOR INITIAL PAVEMENT MARKINGS WITHIN PUBLIC STREETS.
- ALL STRIPING/MARKING MATERIALS SHOULD CONFORM TO SCDOT STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL PAINT SHALL BE SCDOT APPROVED AND SUITABLE FOR APPLICATION ON NEW ASPHALT - MINIMUM 2 COATS.
- BIKE LANES SYMBOLS, ARROWS, ETC SHALL BE SPACED AND PLACED ACCORDING TO THE MUTCD, SCDOT HIGHWAY & SCDOT SUPPLEMENT TO THE MUTCD DESIGN MANUAL.

GENERAL NOTES

- CONTRACTOR IS FULLY RESPONSIBLE FOR THE COORDINATION OF THE DIFFERENT PARTS OF THE PROJECT AND HOW THEY FIT TOGETHER. FAILURE TO COORDINATE BY THE GENERAL CONTRACTOR WILL NOT BE REASON FOR CHANGE ORDER FOR WORK THAT HAS TO BE REDONE.
- THE CONTRACTOR SHALL COORDINATE PLAN SHEETS WITH DETAIL SHEETS. THERE ARE ITEMS SHOWN ON THE DETAIL SHEETS THAT ARE REQUIRED BUT ARE NOT NECESSARILY SHOWN ON THE PLAN SHEETS AND IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE THESE ITEMS AND MAKE SURE THEY GET INSTALLED.
- CONTRACTOR WILL BE FULLY RESPONSIBLE FOR KEEPING EXISTING FACILITIES IN FULL SERVICE WHILE HE COMPLETES HIS WORK. SHOULD WORK NEED TO BE DONE THAT WOULD REQUIRE THE SHUTDOWN OF SUCH FACILITIES, THEN IT SHALL BE DONE AFTER HOURS, AT NIGHT, OR ON WEEKENDS AT NO ADDITIONAL COST TO THE OWNER
- THE SOILS REPORT PERFORMED FOR THIS PROJECT BY AND THROUGH THE OWNER IS AN INTEGRAL PART OF THE DESIGN AND THE CONTRACTOR SHALL READ AND FOLLOW ITS RECOMMENDATIONS TO THE FULLEST EXTENT AS IF THEY WERE PART OF THESE PLANS.
- THERE ARE SEVERAL COMPONENTS THAT COMPRISE THIS PROJECT AND EACH ARE RELATIVE TO HOW THE PROJECT IS TO BE BUILT. THESE ARE THE CONSTRUCTION PLANS, THE CONSTRUCTION DETAILS, THE SPECIFICATIONS, AND SOILS REPORT, WHEN PROVIDED. FAILURE BY THE CONTRACTOR TO COORDINATE ALL OF THESE ITEMS MAY RESULT IN WORK THAT HAS TO BE REMOVE AND REDONE AT THE CONTRACTOR'S SOLE EXPENSE.
- THE MATERIALS SPECIFIED SHALL BE NEW AND OF THE QUALITY CALLED OUT IN THE DRAWINGS AND SPECIFICATIONS. THE CONTRACTOR SHALL PROVIDE THE EXACT MATERIALS AND PLACE THEM IN ACCORDANCE WITH THE DOCUMENTS AND REGULATORY AGENCY REQUIREMENTS.
- REFERENCE TO REGULATORY REQUIREMENTS AND SPECIFICATIONS SHALL MEAN THEY ARE AS MUCH A PART OF THIS DESIGN AS THOSE THAT ARE IN THE PLANS AND SPECOS AND SHALL BE FOLLOWED AS IF THEY WERE FULLY ENUMERATED IN THOSE DOCUMENTS. IF THERE IS A CONFLICT BETWEEN THESE DOCUMENTS AND THOSE OF THE REGULATORY AGENCY, THEN THE MORE RESTRICTIVE OF THE TWO SHALL GOVERN THE CONSTRUCTION UNLESS THE REQUIREMENTS ARE WAIVED BY THE OWNER BY PROVIDING A LETTER FROM THE OWNER EXCEPTING DEVIATION FROM THE PLANS AND ASSUMING ALL RISKS ASSOCIATED THERE WITH THE USE OF SUCH DEVIATIONS
- THE EXECUTION OF A CONTRACT SHALL BE CONCLUSIVE EVIDENCE THAT THE CONTRACTOR HAS INVESTIGATED THE SITE AND IS SATISFIED AS TO THE CONDITIONS TO BE ENCOUNTERED, AS TO THE CHARACTER, QUALITY, AND QUANTITIES OF WORK TO BE PERFORMED AND MATERIALS TO BE FURNISHED, AND AS TO THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIRING A PRIVATE UTILITY LOCATOR TO LOCATE UTILITIES BEFORE CONSTRUCTION BEGINS.
- PROOF-ROLLING SHALL BE PERFORMED WITH A LOADED TANDEN AXLE DUMP TRUCK. WEIGHT TICKET SHOWING THE CURRENT LOAD COMING FROM A SC CERTIFIED SCALE MASTER MUST BE PROVIDED AT BEGINNING OF PROOF ROLL INSPECTION. WEIGHT TICKET CANNOT HAVE BEEN ISSUED MORE THAN ONE HOUR PRIOR TO BEGINNING PROOF ROLL INSPECTION. GROSS VEHICLE WEIGHT SHALL BE NO LESS THAN 65,000-LBS. IN LIEU OF THIS REQUIREMENT CONTRACTOR MAY PROVIDE TANDEN DUMP TRUCK LOADED WITH GBC WITH ZERO FREEBARD. LOAD MUST BE DUMPED ON-SITE AT THE END OF THE PROOF ROLL FOR INSPECTION BY THE ENGINEER. ANY MATERIAL FOUND OTHER THAN GBC WILL RESULT IN AN INSPECTION FAILURE.
- PROOF-ROLLING WILL BE REQUIRED ON THE LANE ADDITIONS, FOR EACH SIDE OF THE ROADWAY AND UNDER THE PROPOSED NEW CURB-LINE, FOR THE SUBGRADE AND BASE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING CONSTRUCTION SIGNAGE, FENCING AND NECESSARY BARRICADES TO BLOCK VEHICULAR TRAFFIC FROM ENTERING THE CONSTRUCTION ZONE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR HIRING A UTILITY LOCATOR.
- TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER AND ENGINEER PRIOR TO CONSTRUCTION



Report of Geotechnical Exploration
CCU Woods Residence Halls Parking Lot EE
 Conway, South Carolina
 S&ME Project No. 1463-18-054

Table 2: Recommended Minimum Pavement Sections^(a)

Pavement Area	Minimum Theoretical Allowable Traffic Load (ESALs)	HMA Surface Course Type C (inches)	HMA Intermediate Course Type C (inches)	Compacted SCDOT Graded Aggregate Base Course (GABC) (inches)
Light-Duty Flexible Pavement - Cars only	177,500	2.5	---	6.0
Heavy-Duty Flexible Pavement - Cars, trucks, and buses	568,500	1.5	2.0	6.0

(a) Single-stage construction and soil compaction as recommended is assumed; S&ME, Inc. must observe pavement subgrade preparation and pavement installation operations.

Pavement Section Design and Construction (Full-Depth Reclamation Option)

Based on our data collection we believe the site may also be a good candidate for full-depth reclamation. If reclamation is chosen to be performed, we recommend the following:

- Use a qualifications-based selection process for the contractor. This type of work is not routine paving, and requires specialized equipment and expertise to perform properly. The contractor that is selected to perform this work should have significant previous experience performing it, and should own or have ready access to the specialized equipment that is necessary to properly perform this work. Based upon our own experiences, we would be able to identify at least two contractors whom we believe to have the experience and equipment for this type of work, if desired.
- The recommended reclamation depth is at least 8 inches. A greater reclamation depth (~10 inches) may be required if a small net increase in final grade elevation cannot be tolerated. Since the materials within the upper 8 inches are relatively high strength materials consisting of asphaltic concrete, coquina base, and sand, a properly cement-stabilized layer that is 8 inches thick should provide the necessary support for new pavement section construction.
- The recommended cement-stabilized base course design compressive strength is about 400 psi; the recommended cement content to achieve this strength should be determined by performing the following: Perform the Standard Method of Test for *Sampling, Preparing and Testing of Cement Modified Recycled Base Compression Specimens in the Laboratory*, using South Carolina Department of Transportation (SCDOT) test procedure SC-T-26. The desired design compressive strength of 400 psi was selected based upon our experience with other reclamation projects. This degree of strength is typically considered high enough to allow strong base support for the pavement to be developed, but not so high as to cause excessive shrinkage, which can lead to cracking that can damage the HMA overlay. This degree of strength also typically results in a reasonable cement demand on the order of roughly 4 percent by weight. S&ME can perform this mix design testing service.

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Castles Engineering

GENERAL NOTES

CCU PARKING LOT EE RESURFACING
STATE PROJECT #H17-N120-MJ

for
 COASTAL CAROLINA UNIVERSITY
 CITY OF CONWAY, Horry County, SOUTH CAROLINA

SCALE: N.T.S.

DESIGNED BY: JRP

DRAWN BY: TCM

CHECKED BY: RLC

DATE ISSUED: 07/15/19

Number	Date	Revisions

SOUTH CAROLINA
 REGISTERED PROFESSIONAL ENGINEER
 No. 000350
 STATE OF SOUTH CAROLINA

CASTLES CONSULTING ENGINEERS, INC.

SOUTH CAROLINA
 REGISTERED PROFESSIONAL ENGINEER
 No. 8826
 STATE OF SOUTH CAROLINA

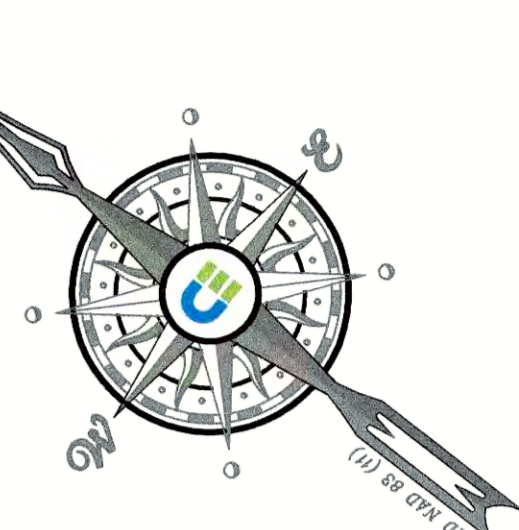
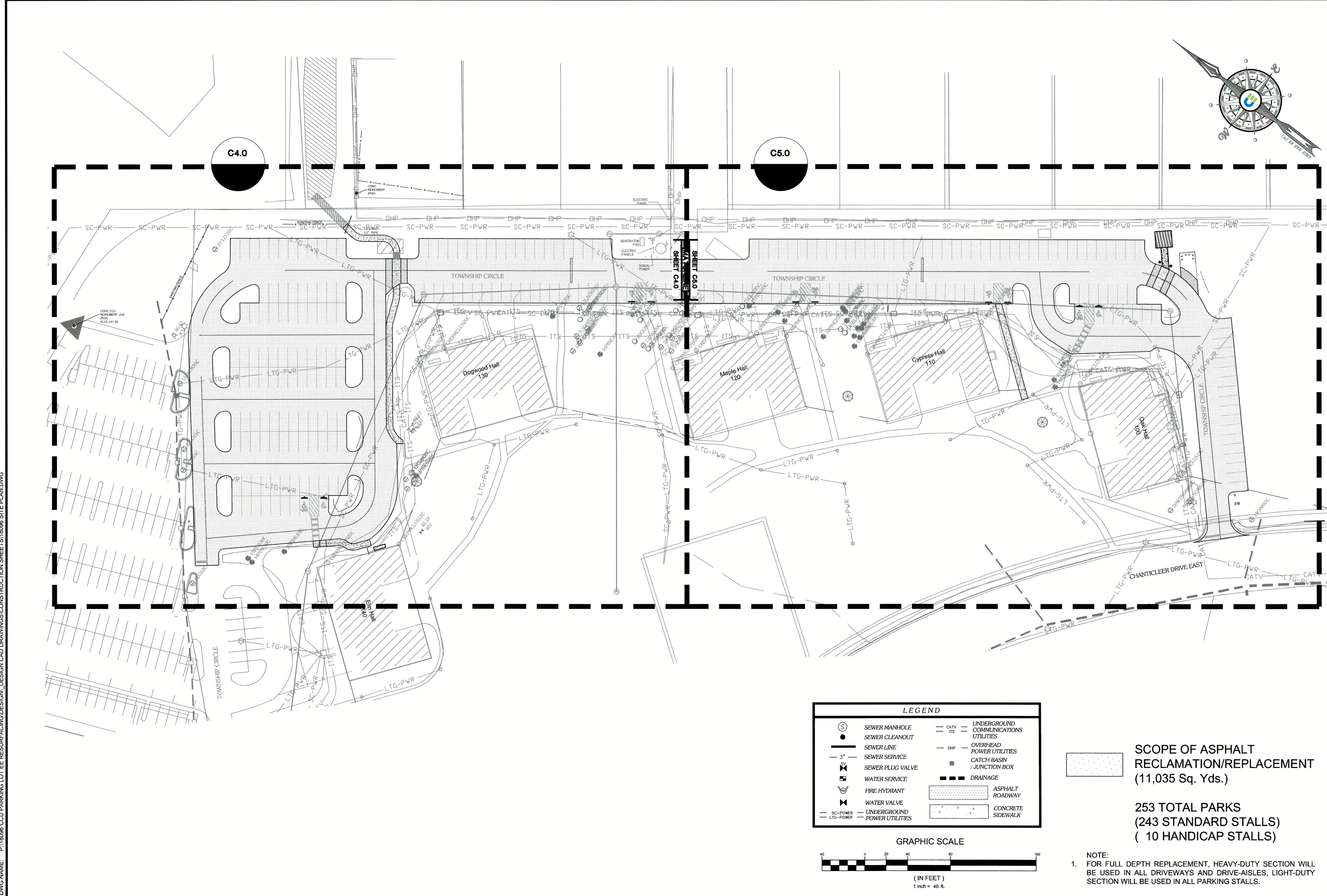
ROBERT L. CASTLES

SHEET NUMBER:

C2.0

STATE PN#: H17-N120-MJ
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DWG NAME: P:\18096 CCU PARKING LOT EE RESURFACING\DESIGN CAD DRAWINGS\CONSTRUCTION SHEETS\18096 SITE PLAN.DWG

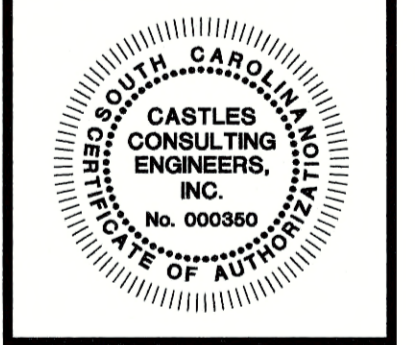


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OVERALL KEY PLAN
CCU PARKING LOT EE RESURFACING
STATE PROJECT #H17-N120-MJ
for
COASTAL CAROLINA UNIVERSITY
CITY OF CONWAY, Horry County, SOUTH CAROLINA

SCALE:	1:40
DESIGNED BY:	JRP
DRAWN BY:	TCM
CHECKED BY:	RLC
DATE ISSUED:	07/15/19

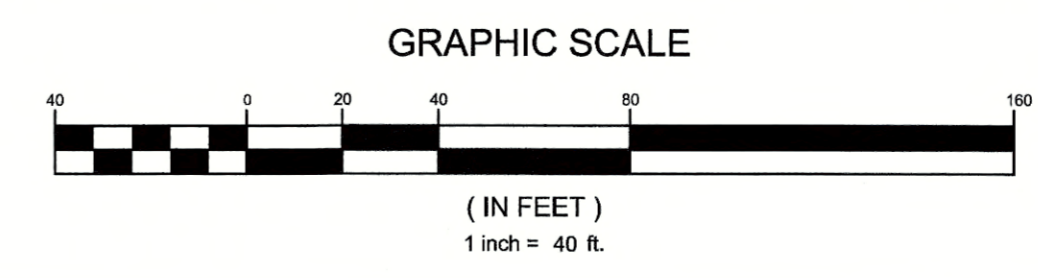
Number	Date	Revisions



SHEET NUMBER:
C3.0
STATE PN#: H17-N120-MJ
PROJECT#: 18.096

LEGEND

	SEWER MANHOLE		UNDERGROUND COMMUNICATIONS UTILITIES
	SEWER CLEANOUT		OVERHEAD POWER UTILITIES
	SEWER LINE		CATCH BASIN / JUNCTION BOX
	SEWER SERVICE		DRAINAGE
	SEWER PLUG VALVE		ASPHALT ROADWAY
	WATER SERVICE		CONCRETE SIDEWALK
	FIRE HYDRANT		
	WATER VALVE		
	SC-PWR		
	LTG-PWR		
	CATV		
	ITS		

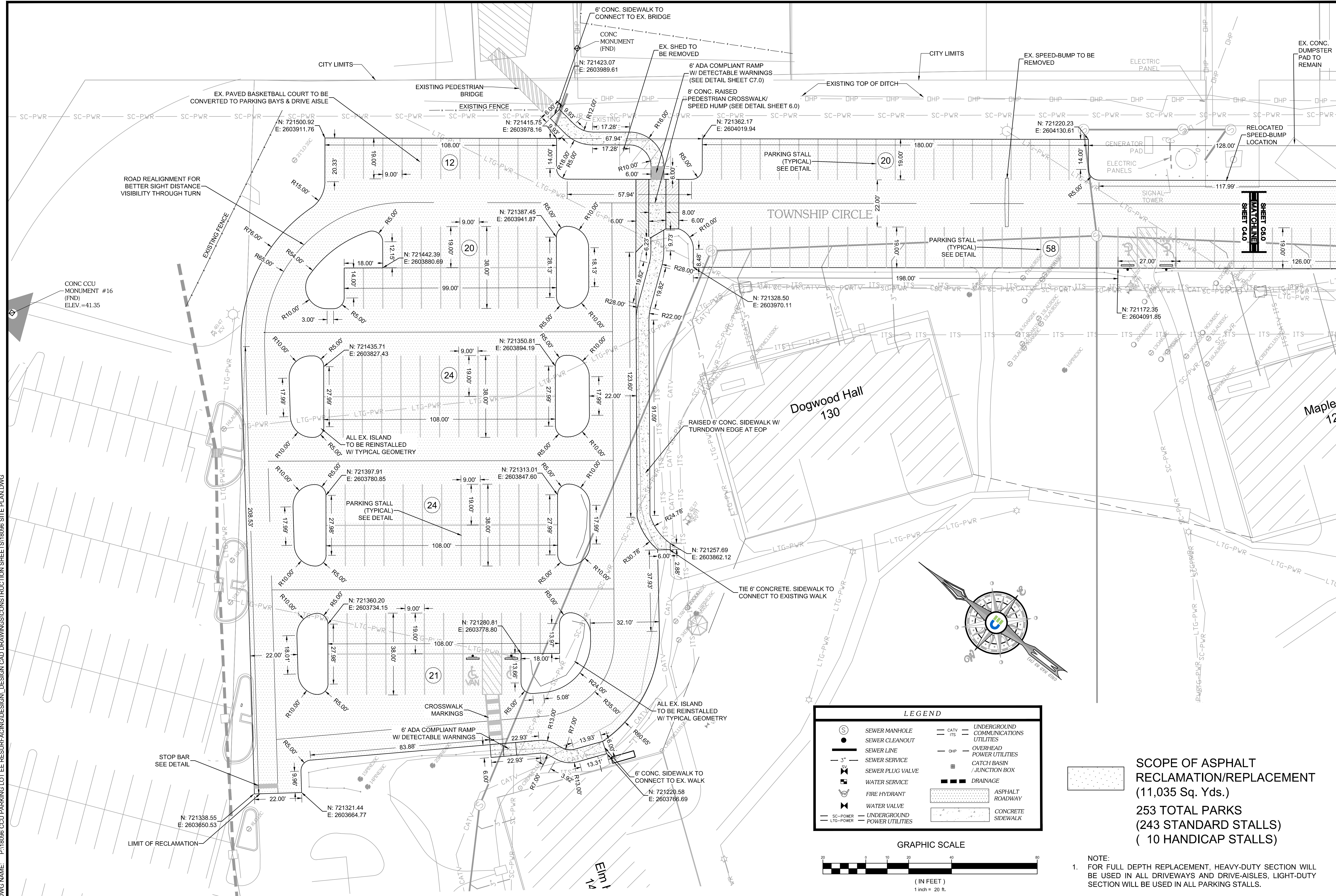


SCOPE OF ASPHALT RECLAMATION/REPLACEMENT (11,035 Sq. Yds.)

253 TOTAL PARKS (243 STANDARD STALLS) (10 HANDICAP STALLS)

NOTE:
1. FOR FULL DEPTH REPLACEMENT, HEAVY-DUTY SECTION WILL BE USED IN ALL DRIVEWAYS AND DRIVE-AISLES, LIGHT-DUTY SECTION WILL BE USED IN ALL PARKING STALLS.

DWG NAME: P118095 CCU PARKING LOT EE RESURFACING/CONSTRUCTION SHEET S118095 SITE PLAN.DWG

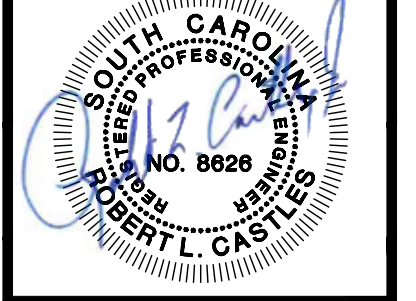
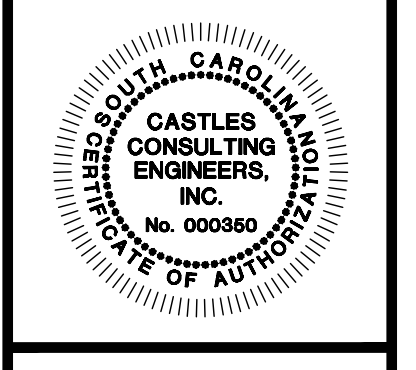


ENGINEERING/PLANNING
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2024 Corporate Offices: Drive 102
Myrtle Beach, SC 29577
Telephone: 843.443.0910
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www.castlesengineering.com

SITE PLAN 1
CCU PARKING LOT EE RESURFACING
STATE PROJECT #H17-N120-MJ
for
COASTAL CAROLINA UNIVERSITY
CITY OF CONWAY, HORRY COUNTY, SOUTH CAROLINA

SCALE: 1:20
DESIGNED BY: JRP
DRAWN BY: TCM
CHECKED BY: JRP
DATE ISSUED: 07/15/19

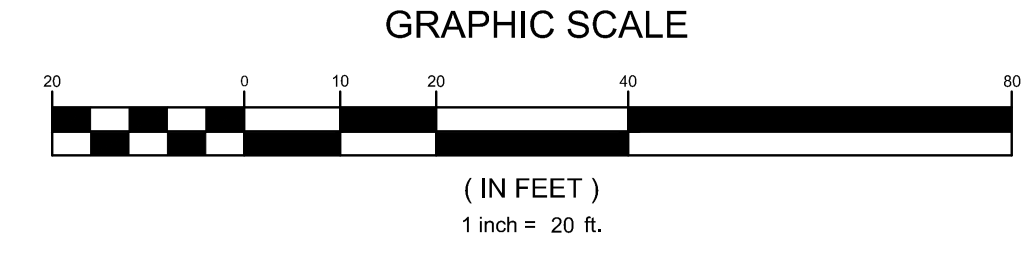
Number	Date	Revisions



SHEET NUMBER:
C4.0
STATE P/N#: H17-N120-MJ
PROJECT#: 18.096

LEGEND

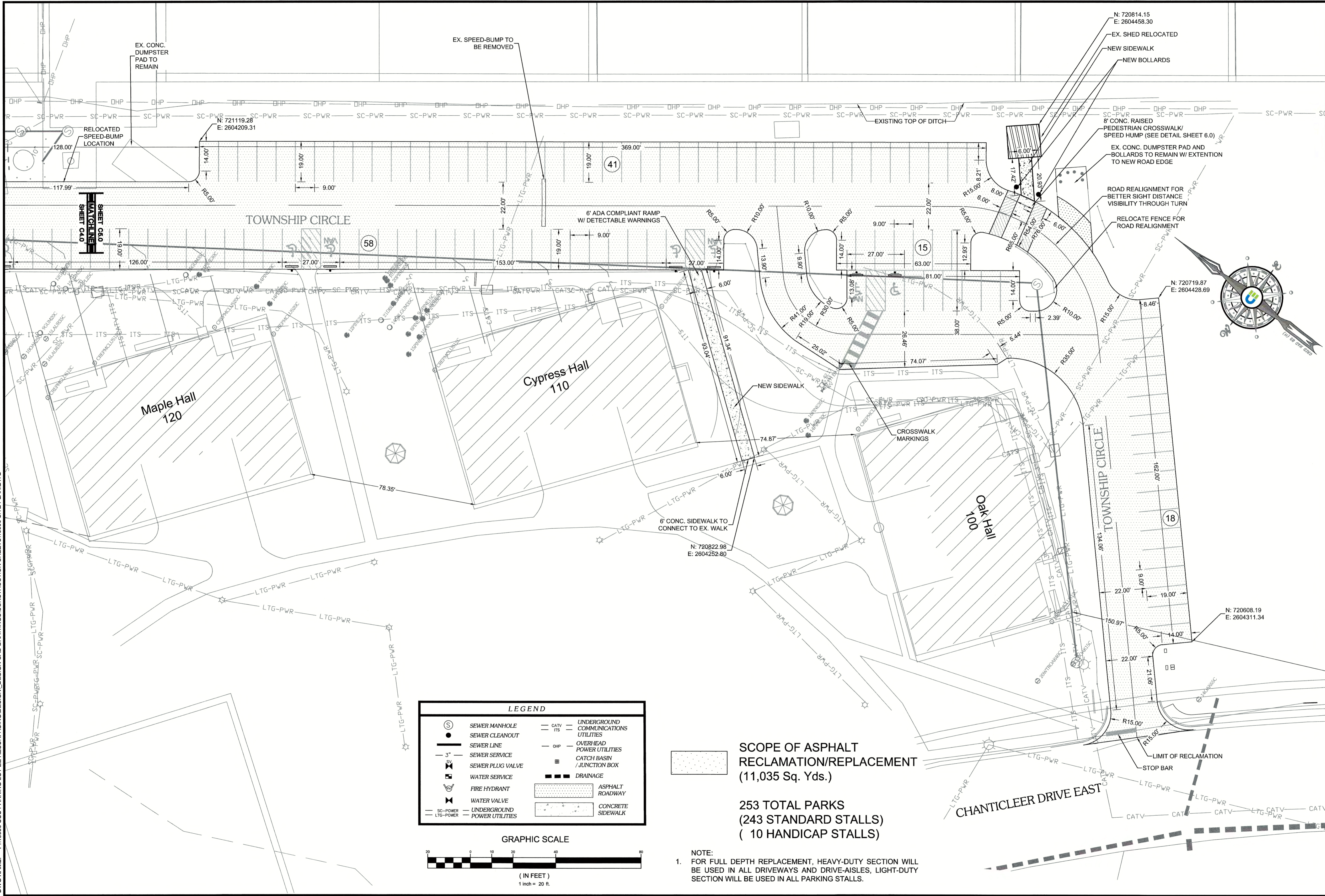
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	SEWER CLEANOUT		OVERHEAD POWER UTILITIES
	SEWER LINE		CATCH BASIN / JUNCTION BOX
	SEWER SERVICE		DRAINAGE
	SEWER PLUG VALVE		ASPHALT ROADWAY
	WATER SERVICE		CONCRETE SIDEWALK
	FIRE HYDRANT		
	WATER VALVE		
	UNDERGROUND POWER UTILITIES		



SCOPE OF ASPHALT RECLAMATION/REPLACEMENT
(11,035 Sq. Yds.)
253 TOTAL PARKS
(243 STANDARD STALLS)
(10 HANDICAP STALLS)

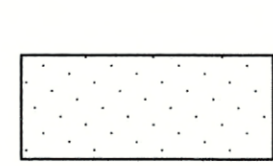
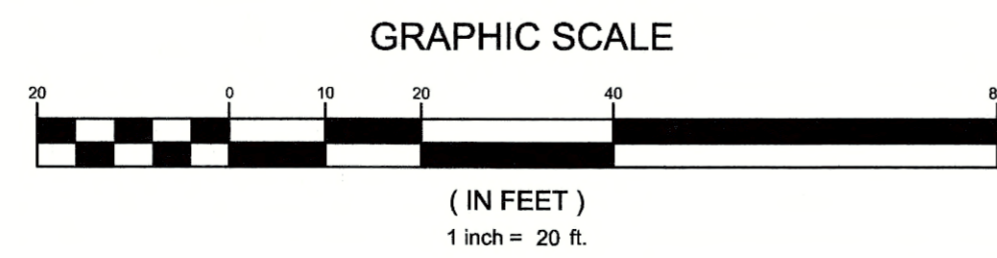
NOTE:
1. FOR FULL DEPTH REPLACEMENT, HEAVY-DUTY SECTION WILL BE USED IN ALL DRIVEWAYS AND DRIVE-AISLES, LIGHT-DUTY SECTION WILL BE USED IN ALL PARKING STALLS.

DWG NAME: P:18096 CCU PARKING LOT EE RESURFACING DESIGN CAD DRAWINGS CONSTRUCTION SHEETS 18096 SITE PLAN DWG



LEGEND

	SEWER MANHOLE		UNDERGROUND COMMUNICATIONS UTILITIES
	SEWER CLEANOUT		OVERHEAD POWER UTILITIES
	SEWER LINE		CATCH BASIN / JUNCTION BOX
	SEWER SERVICE		DRAINAGE
	SEWER PLUG VALVE		ASPHALT ROADWAY
	WATER SERVICE		CONCRETE SIDEWALK
	FIRE HYDRANT		
	WATER VALVE		
	UNDERGROUND POWER UTILITIES		
	LTG-PWR		



SCOPE OF ASPHALT RECLAMATION/REPLACEMENT
(11,035 Sq. Yds.)

253 TOTAL PARKS
(243 STANDARD STALLS)
(10 HANDICAP STALLS)

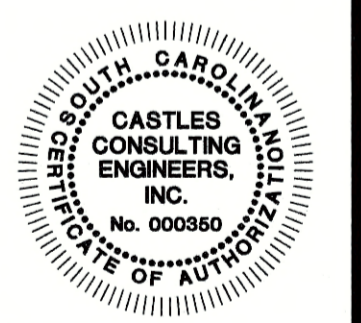
NOTE:
1. FOR FULL DEPTH REPLACEMENT, HEAVY-DUTY SECTION WILL BE USED IN ALL DRIVEWAYS AND DRIVE-AISLES, LIGHT-DUTY SECTION WILL BE USED IN ALL PARKING STALLS.

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SITE PLAN 2
CCU PARKING LOT EE RESURFACING
STATE PROJECT #H17-N120-MJ
CASTLES ENGINEERING, INC.
COASTAL CAROLINA UNIVERSITY
CITY OF CONWAY, Horry County, SOUTH CAROLINA

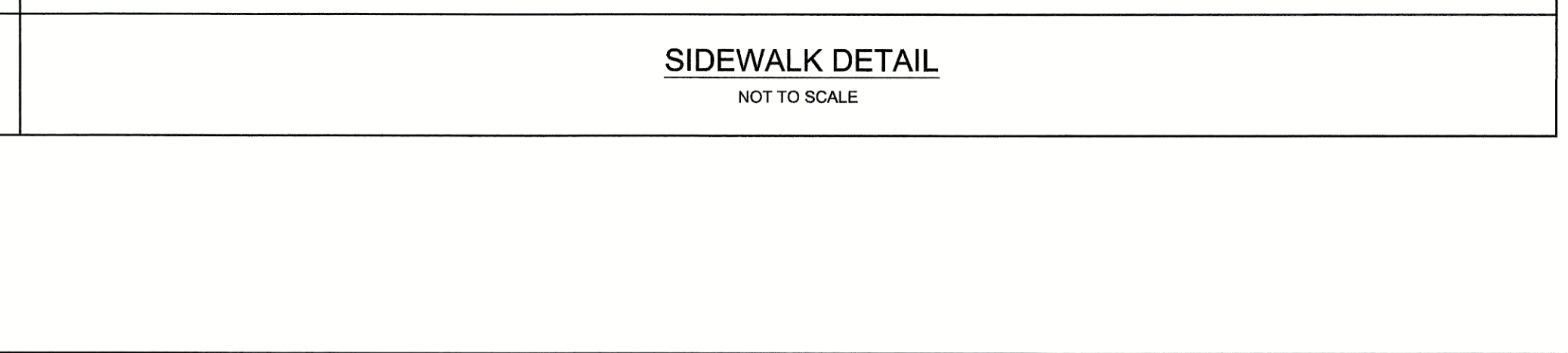
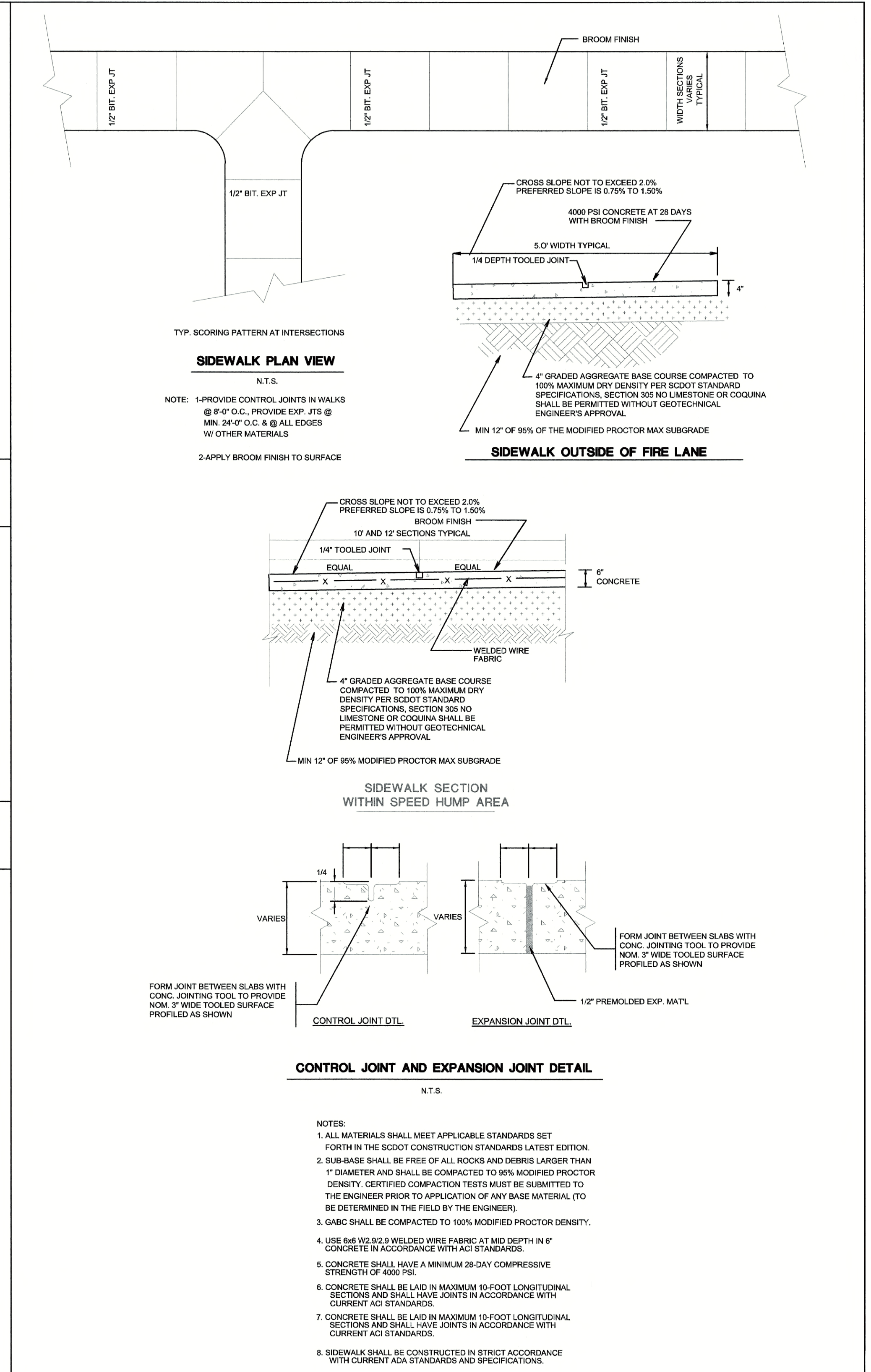
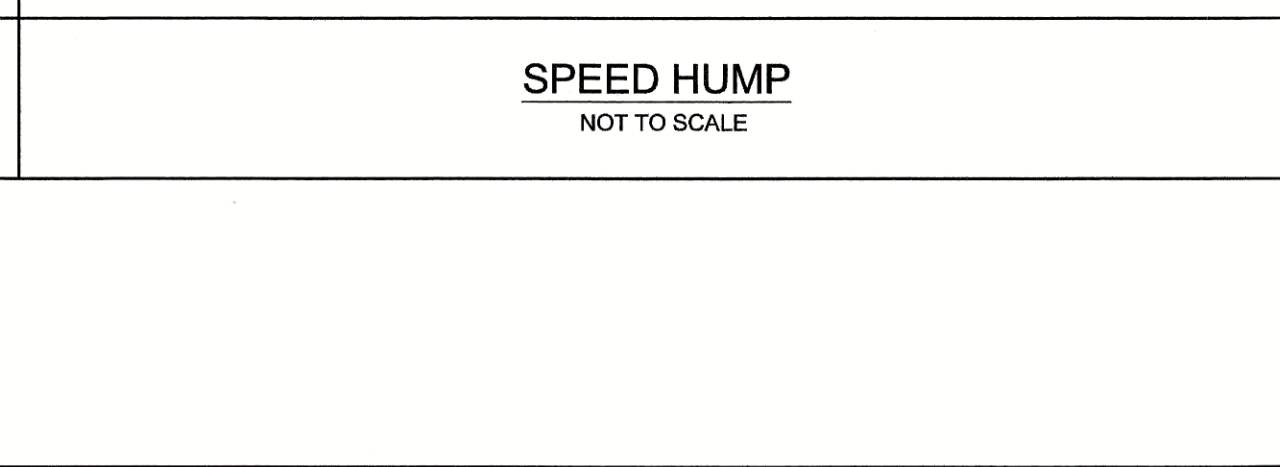
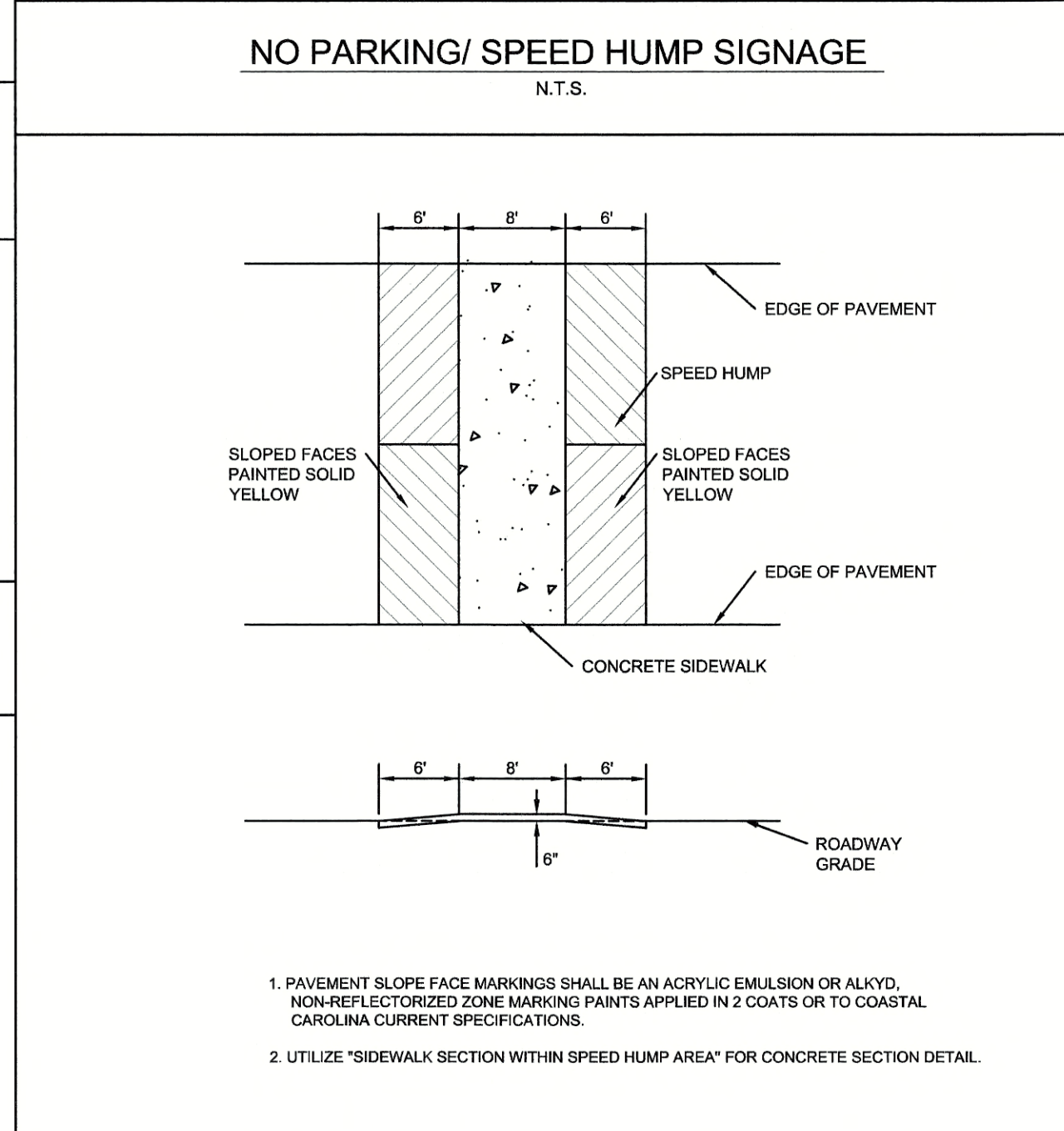
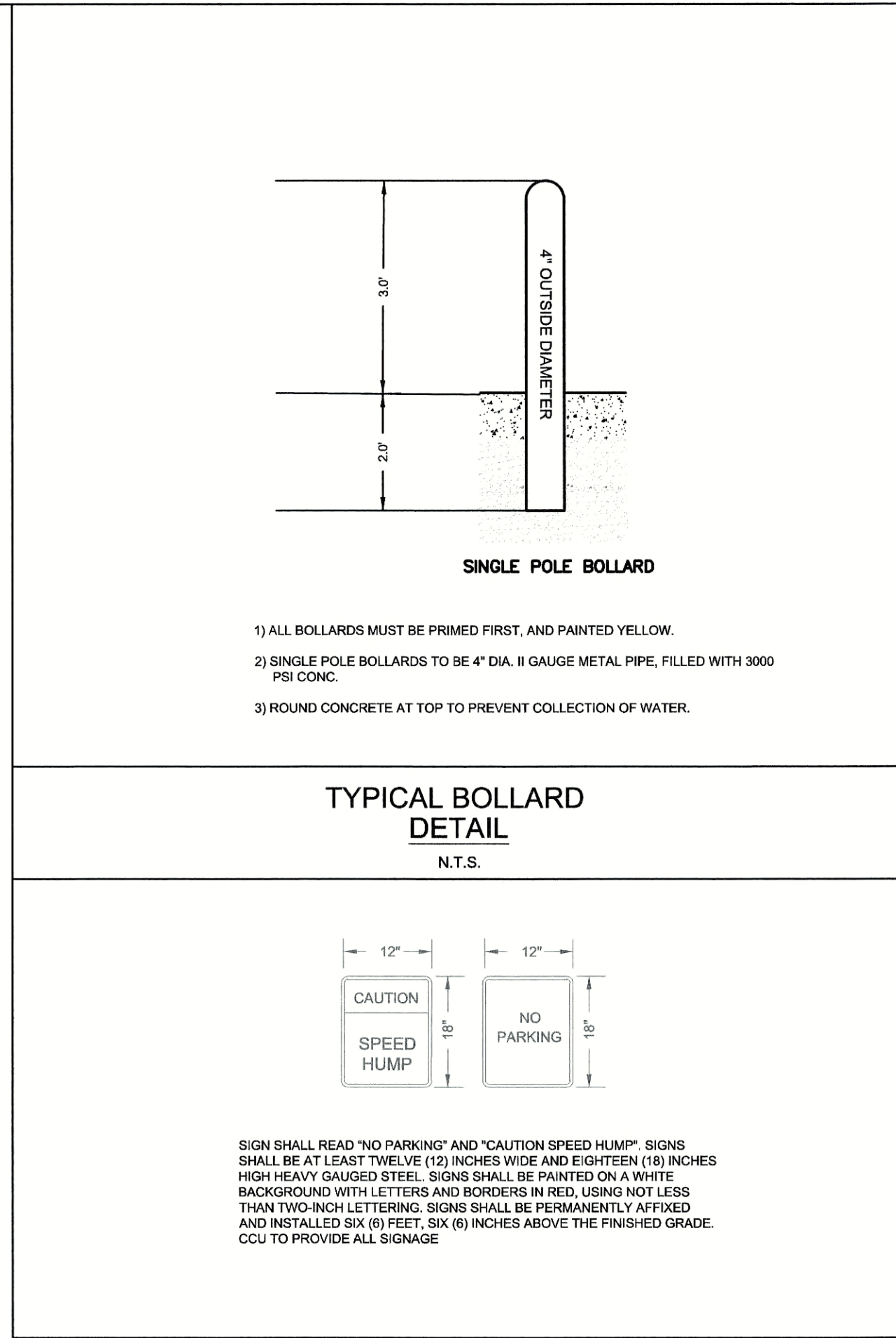
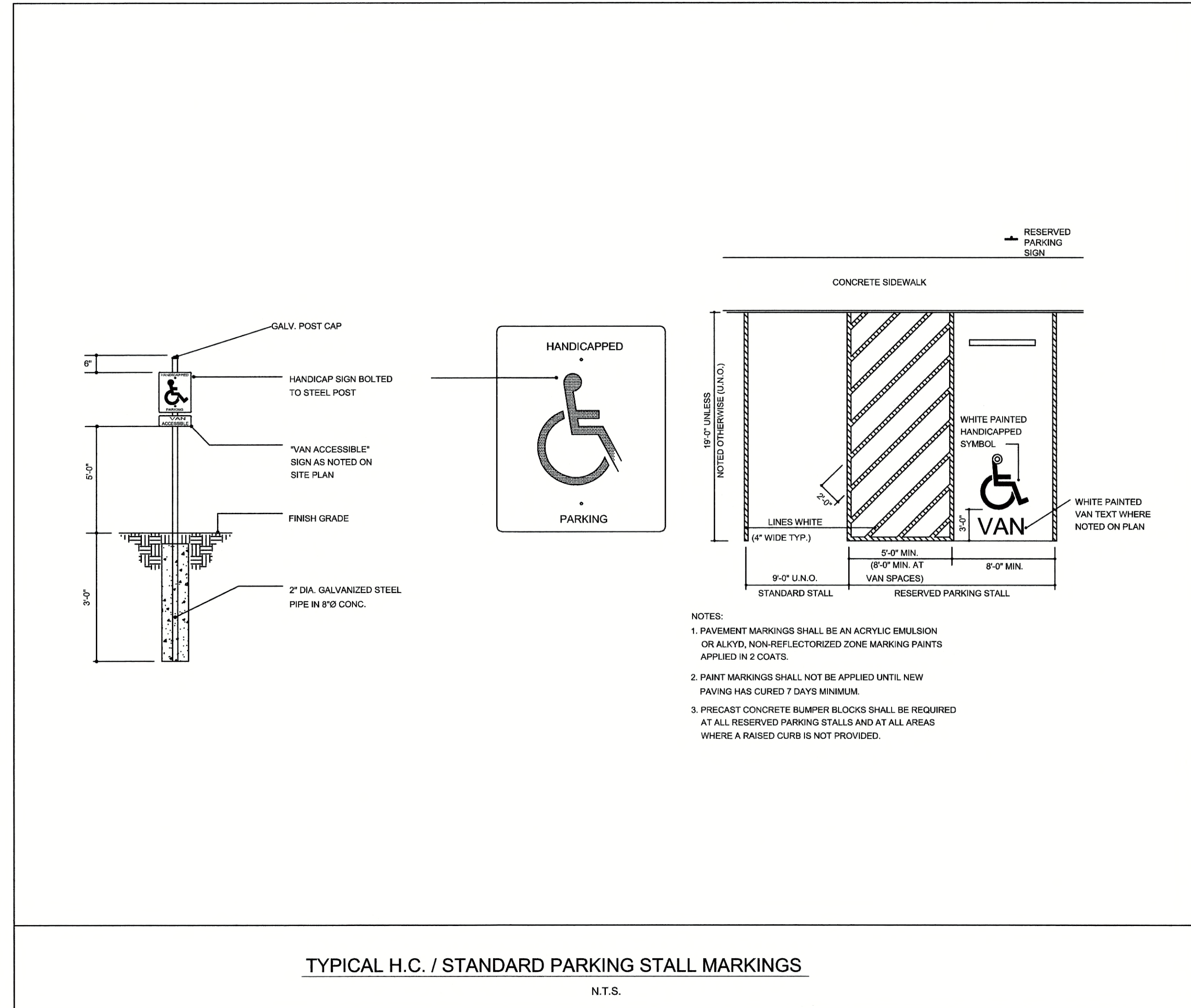
SCALE:	1:20
DESIGNED BY:	JRP
DRAWN BY:	TCM
CHECKED BY:	RLC
DATE ISSUED:	07/15/19

Number	Date	Revisions



SHEET NUMBER:
C5.0
STATE PN#: H17-N120-MJ
PROJECT#: 18.096

DWG NAME: P:\18096 CCU PARKING LOT EE RESURFACING\CONSTRUCTION SHEETS\18096 COVER, NOTES, AND DETAILS.DWG

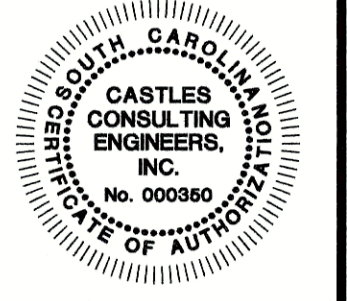


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DETAILS
CCU PARKING LOT EE RESURFACING
STATE PROJECT #H17-N120-MJ
 COASTAL CAROLINA UNIVERSITY
 CITY OF CONWAY, Horry County, SOUTH CAROLINA

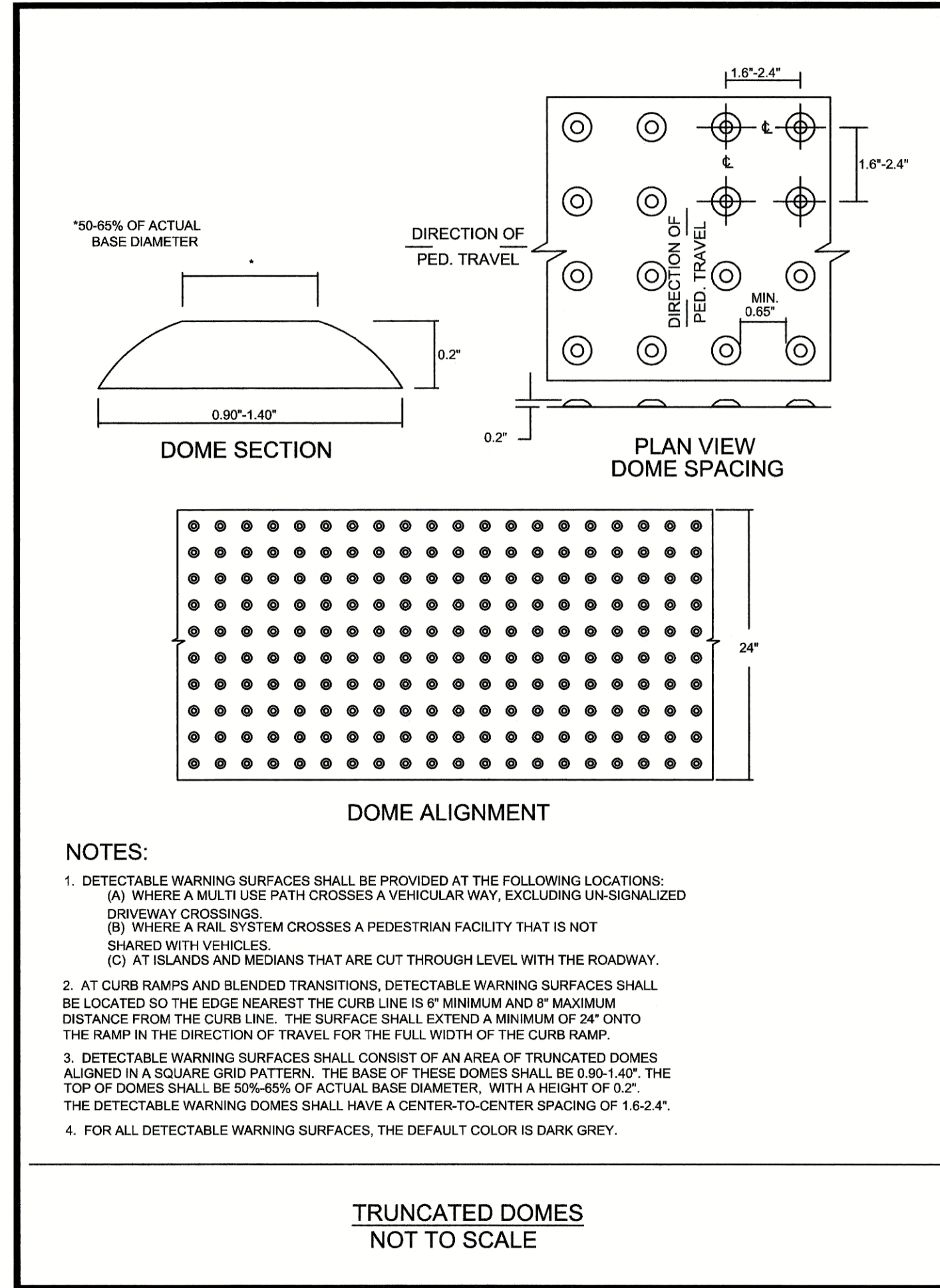
SCALE: N.T.S.
 DESIGNED BY: JRP
 DRAWN BY: TCM
 CHECKED BY: RLC
 DATE ISSUED: 07/15/19

Number	Date	Revisions
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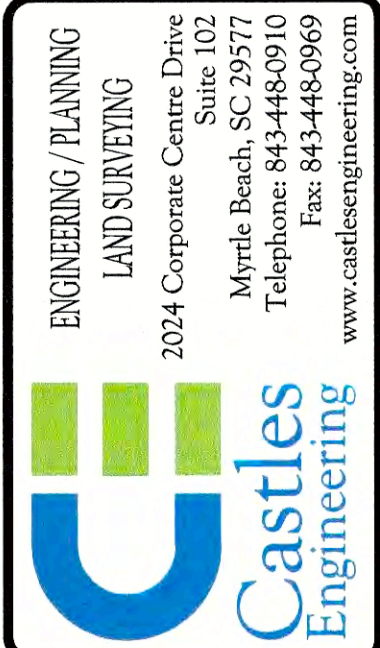
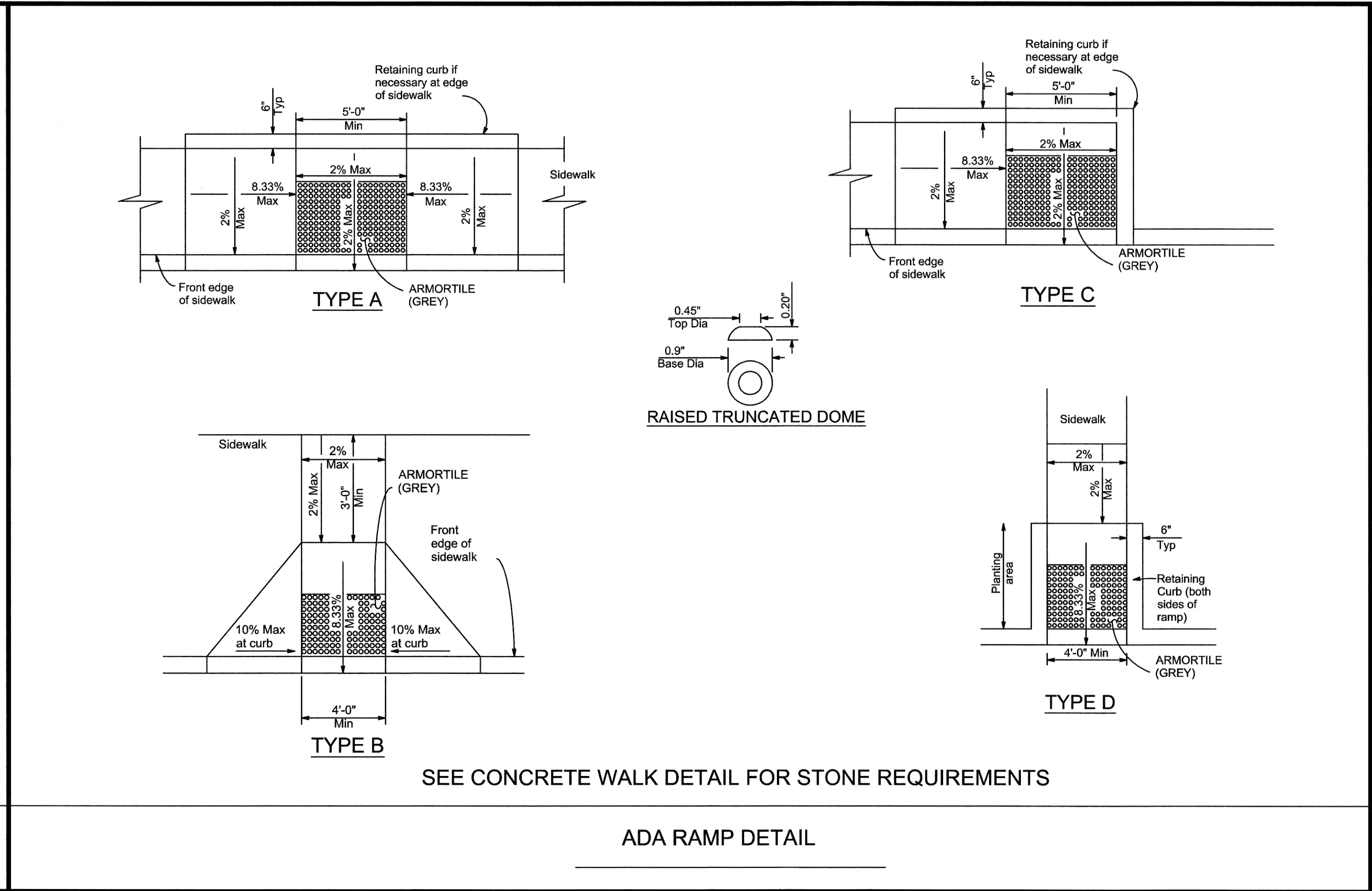


SHEET NUMBER:
C6.0
 STATE PNR#: H17-N120-MJ
 PROJECT#: 18.096

DWG NAME: P:\18096 CCU PARKING LOT EE RESURFACING\DESIGN CAD DRAWINGS\CONSTRUCTION SHEETS\18096 COVER, NOTES, AND DETAILS.DWG



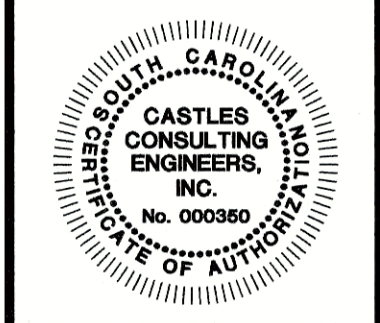
- NOTES:**
- DETECTABLE WARNING SURFACES SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS:
 - (A) WHERE A MULTI USE PATH CROSSES A VEHICULAR WAY, EXCLUDING UN-SIGNALIZED DRIVEWAY CROSSINGS
 - (B) WHERE A RAIL SYSTEM CROSSES A PEDESTRIAN FACILITY THAT IS NOT SHARED WITH VEHICLES.
 - (C) AT ISLANDS AND MEDIANS THAT ARE CUT THROUGH LEVEL WITH THE ROADWAY.
 - AT CURB RAMPS AND BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL BE LOCATED SO THE EDGE NEAREST THE CURB LINE IS 6" MINIMUM AND 6" MAXIMUM DISTANCE FROM THE CURB LINE. THE SURFACE SHALL EXTEND A MINIMUM OF 24" ONTO THE RAMP IN THE DIRECTION OF TRAVEL FOR THE FULL WIDTH OF THE CURB RAMP.
 - DETECTABLE WARNING SURFACES SHALL CONSIST OF AN AREA OF TRUNCATED DOMES ALIGNED IN A SQUARE GRID PATTERN. THE BASE OF THESE DOMES SHALL BE 0.90-1.40". THE TOP OF DOMES SHALL BE 50%-65% OF ACTUAL BASE DIAMETER, WITH A HEIGHT OF 0.2". THE DETECTABLE WARNING DOMES SHALL HAVE A CENTER-TO-CENTER SPACING OF 1.6-2.4".
 - FOR ALL DETECTABLE WARNING SURFACES, THE DEFAULT COLOR IS DARK GREY.



DETAILS
CCU PARKING LOT EE RESURFACING
STATE PROJECT #H17-N120-MJ
for
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CITY OF CONWAY, Horry County, SOUTH CAROLINA

SCALE:	N.T.S.
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SHEET NUMBER:
C7.0
STATE P/N#: H17-N120-MJ
PROJECT#: 18.096